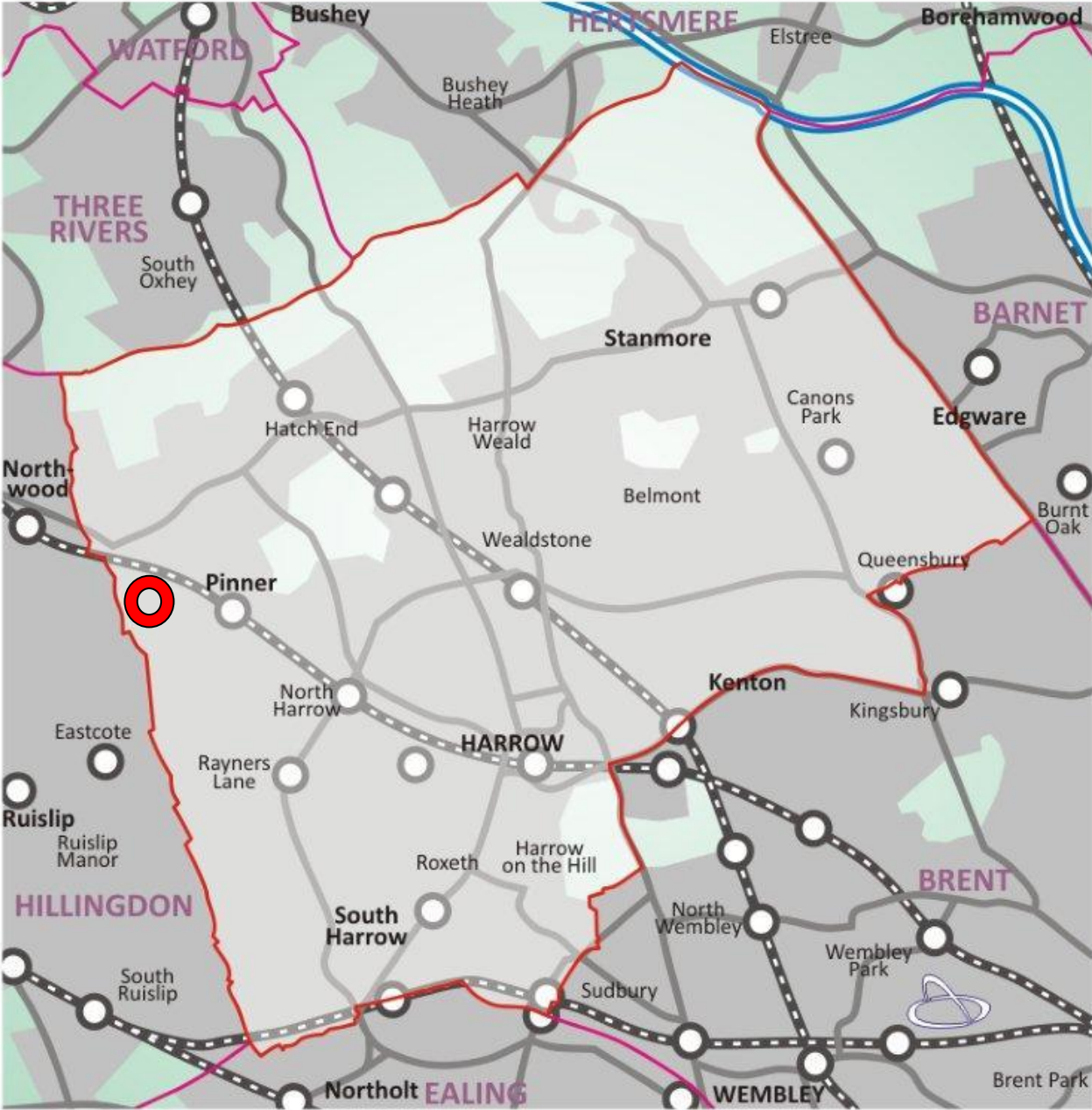


 = application site



21B, 29-31 & 33-35 Bridge Street, Pinner, HA5 3HR	P/4965/21
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# SITE PLAN



# LONDON BOROUGH OF HARROW

## PLANNING COMMITTEE

20<sup>th</sup> JULY 2022

APPLICATION NUMBER: P/4965/21  
VALIDATE DATE: 11/01/2022  
LOCATION: 21B & 29-31 BRIDGE STREET, PINNER  
WARD: PINNER  
POSTCODE: HA5 3HR  
APPLICANT: C/P AGENT  
AGENT: THE HARRIS PARTNERSHIP  
CASE OFFICER: SELINA HOTWANI  
EXPIRY DATE: 15/02/2022

### PROPOSAL

Creation of third floor comprising of four additional flats (1 x 2 bed and 3 x 1 bed); installation of access staircase at rear; bin and cycle stores; external alterations

### RECOMMENDATION

The Planning Committee is asked to:

### RECOMMENDATION A

- 1) Agree the reasons for approval as set out in this report, and
- 2) Grant planning permission subject to authority being delegated to the Interim Chief Planning Officer in consultation with the Director of Legal and Governance Services for the completion of the Section 106 legal agreement and other enabling legislation and issue of the planning permission and subject to minor amendments to the conditions (set out in Appendix 1 of this report) or the legal agreement. The Section 106 Agreement Heads of Terms would cover the following matters:
  - i. Parking Permit Restrictions – The development to be “resident permit restricted” in accordance with section 16 of the GLC (Gen Powers) Act 1974 and the developer to ensure that 1) all marketing/advertising material makes reference to this fact and 2) all agreements contain a covenant to the effect that future occupiers and tenants (other than those who are registered disabled) will not be entitled to apply for residents parking permit or a visitor permit.
  - ii. A contribution of £1,500 in accordance with the adopted fees and charges is required to amend the Traffic Management Order and a monitoring fee of £500.
  - iii. Legal Fees: Payment of Harrow Council’s reasonable costs in the preparation of the legal agreement.

## **RECOMMENDATION B**

That if the Section 106 Agreement is not completed by 27<sup>th</sup> October 2022, or as such extended period as may be agreed by the Interim Chief Planning Officer, then it is recommended to delegate the decision to **REFUSE** planning permission to the Interim Chief Planning Officer on the grounds that: The proposed development, in the absence of a Legal Agreement to provide appropriate improvements, benefits and monitoring that directly relate to the development, would fail to adequately mitigate the impact of the development on the wider area and provide for necessary physical infrastructural improvements arising directly from the development, contrary to the National Planning Policy Framework (2021), policies T3 and DF1 of The London Plan (2021), Core Strategy (2012) policy CS1, and policies DM1, DM43 and DM50 of the Harrow Development Management Policies Local Plan (2013) and the Supplementary Planning Document: Planning Obligations & Affordable Housing (2013).

## **REASON FOR THE RECOMMENDATIONS**

The proposed development would result in a modest extension to the existing building within an area benefiting from excellent transport links, that responds positively to the local context, and would provide appropriate living conditions for future occupiers. The site would continue to provide an active retail function adjacent to the Town Centre, as well as providing for residential accommodation which would increase the vibrancy of the adjoining town centre and housing choice within the borough.

## **INFORMATION**

This application is reported to Planning Committee as it would provide in excess of 3 new residential units. The application is therefore referred to the Planning Committee as it does not fall within any of the provisions set out at paragraphs 1(a) – 1(h) of the Scheme of Delegation dated 12th December 2018.

Statutory Return Type:	Minor/Other
Council Interest:	N/A
GLA Community Infrastructure Levy:	£11,802
(CIL) Contribution (provisional):	
Local CIL requirement:	£30,291.80

## **HUMAN RIGHTS ACT**

The provisions of the Human Rights Act 1998 have been taken into account in the processing of the application and the preparation of this report.

## **EQUALITIES**

In determining this planning application the Council has regard to its equalities obligations including its obligations under section 149 of the Equality Act 2010.

For the purposes of this application there are no adverse equalities issues.

## **S17 Crime & Disorder Act**

Policy D11 of the London Plan (2021) and Policy DM1 of the Development Management Policies Local Plan require all new developments to have regard to safety and the measures to reduce crime in the design of development proposal. It is considered that the development does not adversely affect crime risk. However, a condition has been recommended for evidence of certification of Secure by Design Accreditation for the development to be submitted to and approved in writing by the Local Planning Authority before any part of the development is occupied or used.

## 1. **SITE DESCRIPTION**

- 1.1 The application site consists of ground floor retail units with office and residential units above. These are identified as 21b, 29-31 and 33-35 Bridge Street. Unit 21b is a storage unit to the rear of the site accessed via the service yard off Chapel Lane. There are two ground floor retail units comprising retail unit A (29-31 Bridge Street) and retail unit B (33-35 Bridge Street). Above unit 29 is 29A comprising first and second floor offices. Above units 31-35 are six residential units at first and second floor levels. Flats 1, 3 and 4 are located at first floor and Flats 2, 5 and 6 at second floor.
- 1.2 An underpass off Bridge Street is located adjacent to retail unit A which also forms the entrance to the upper floor office and residential units as well as a refuse area for the existing flats.
- 1.3 The application site is within the Primary Area and Shopping Frontage within Pinner Town Centre. The underpass area and a small corner of Unit 29 is within EA Flood Zone 2. Directly south of the site is the Pinner High Street Conservation Area.
- 1.4 The premises forms part of a parade of three-storey buildings with commercial uses at ground floor level and residential uses on the upper floors. Retail servicing is from the rear service yard and residential servicing is from Bridge Street.
- 1.5 The adjacent neighbour to the east is in A2 use and to the left is Santander, an A2 use.

## 2. **PROPOSAL**

- 2.1 The proposal seeks to extend the properties upwards to create a third floor to provide four new flats. The scheme seeks to reconfigure and refurbish the existing first and second floor residential units.
- 2.2 A new external staircase accessed from the existing underpass would be created up to the new units in Block B (B5 and B6, above the existing offices at 29a). The new external staircase would lead to the existing first floor entrance to Block A which would be utilised for the proposed new units A5 and A6.
- 2.3 Whilst no external works are proposed in respect of the ground floor retail units, the proposal comprises the internal refurbishment of retail unit A which would reduce the overall retail floor space by 13sqm.
- 2.4 Refuse and cycle stores would also be provided.

## 3. **RELEVANT PLANNING HISTORY**

- 3.1 A summary of the relevant planning application history is set out in the table below:

Ref no.	Description	Status and date of decision
LBH/8952	Installation of a new shopfront	Granted: 15/06/1973

WEST/879/02/FUL	Single-storey rear extension to shop and external staircase and shopfront	Granted: 08/12/2002
P/2677/12	Use of public highway for placing of moveable tables (6) and chairs (12) in connection with coffee shop (Class A1)	Refused: 11/12/2012
<i>The proposal would fail to leave an adequate area of footway available for use by pedestrians, to the detriment of the free flow of pedestrians and highway safety, contrary to policy 6.10 of The London Plan (2011), saved policies D4 and T6 of the Harrow Unitary Development Plan (2004) and Supplementary Planning Document: Access for All (2006).</i>		
P/1487/18	The application is for a Certificate of Lawfulness for a proposed development to change the use of the ground floor unit from retail (Class A1 - shops) to A2 (Class A2 – financial and professional services).	Granted 25/5/2018
P/1449/19	Change of use of ground floor from Coffee shop (Class A1) to Fitness Studio (Class D2); Installation of shop front	Refused: 12/06/2019
<i>Insufficient information has been provided in terms of fluvial flood risk and surface water flooding and how it will demonstrate suitable on site water management and surface water attenuation. The proposal has therefore failed to demonstrate compliance with the National Planning Policy Framework (2019), policy CS1.U of the Harrow Core Strategy (2012) and policy DM9 of the Harrow Development Management Policies Local Plan (2013).</i>		
<b>29-31 Bridge Street</b>		
P/1064/21	Single storey front infill extension; New shop front	Granted 25/10/2021
P/1079/21	New shopfront glazing, relocation of entrance doors and installation of overhead & hanging signage	Under consideration
<b>29A Bridge Street</b>		
P/1846/22/PRIOR	Change of use from Office (Class E) To 2 Self-Contained Flats (Class C3); (Prior Approval Of Transport & Highways Impacts Of The Development, Contamination Risks And Flooding Risks On The Site, and Impacts Of Noise From Commercial Premises On The Intended Occupiers Of The Development and provision of	Under consideration

	natural light in all habitable rooms)	
<b>31-35 Bridge Street</b>		
P/669/06/CFU	Demolition and relocation of existing plant room and construction of 3 storey block of 12 self-contained flats with off street parking and amenity space	Refused 09/06/2006
<p>1. <i>The proposal is an unacceptable overdevelopment of a backland site that by reason of poor siting and layout and excessive site coverage by buildings would be an inappropriate form of residential development in this commercial location, resulting in poor living conditions for future occupiers.</i></p> <p>2. <i>The proposed development by reason of excessive height, scale, bulk, massing and siting would result in unacceptable overlooking and loss of privacy to residents of neighbouring properties.</i></p> <p>3. <i>The proposed development by reason of excessive height, scale, bulk, massing and siting would result in unacceptable loss of daylight and sunlight to neighbouring properties.</i></p> <p>4. <i>Car parking cannot satisfactorily provided within the curtilage of the site to meet the Council's requirements in respect of the development, and the likely increase in parking on the neighbouring highway(s) would be detrimental to the free flow and safety of traffic on the neighbouring highway(s).</i></p> <p>5. <i>The proposed development would prejudice the implementation of Service Road proposal 5 of the Harrow Unitary Development Plan 2004.</i></p>		
P/1696/07	Construction of 12 flats with courtyard	Refused 03/09/2007
<p>1. <i>The proposal is an unacceptable overdevelopment of the site that by reason of poor siting and layout and excessive site coverage. The buildings would be an inappropriate form of residential development in this commercial location, resulting in poor living conditions for future occupiers, contrary to policies SD1, D4, and D5 of the Harrow Unitary Development Plan (2004) and Supplementary Planning Guidance: Designing New Development (March 2003).</i></p> <p>2. <i>The proposed development by reason of excessive height, scale, bulk, massing and siting would result in unacceptable overlooking and loss of privacy, and loss of daylight and sunlight to occupants of neighbouring properties, contrary to policies SD1, D4 and D5 of the Harrow Unitary Development Plan (2004) and Supplementary Planning Guidance: Designing New Development (March 2003).</i></p> <p>3. <i>The proposed development, by way of poor design, layout and inadequate room size, would fail to meet requirements of LifeTime Homes Standards and Wheelchair Homes Standards, contrary to policy H18 of the Harrow Unitary Development Plan 2004 and the Councils Supplementary Planning Document on Accessible Homes (April 2006).</i></p> <p>4. <i>The proposed development, having no on-site parking provision or turning area, would result in an increase in parking on the neighbouring highway to the detriment to the free flow and safety of traffic on the neighbouring highway contrary to policy T13 of the Harrow Unitary Development Plan 2004.</i></p> <p>5. <i>The proposed development, by way of poor design and layout, would fail to meet the key principles of Secured By Design and Safer Places and would create</i></p>		



*opportunities for crime contrary to policy D4 of the Harrow Unitary Development Plan 2004 and Supplementary Planning Guidance: Designing New Development, (March 2003).*

#### **4. CONSULTATION**

- 4.1 A total of 13 consultation letters were sent to neighbouring properties regarding this application. A site notice was posted on 18 January 2022.
- 4.2 The overall public consultation period expired on the 8<sup>th</sup> February 2022.
- 4.3 A total of one response has been received to date.
- 4.4 A summary of the response received from the Pinner Association along with the Officer comments are set out below:

- As the application as described in the Design and Access Statement is in a “prominent position on the middle of Pinner on Bridge Street” the frontage may be viewed from distance from the Love Lane junction and it is important that the design of the proposed additional storey is in harmony with and enhance the existing building on the street frontage.  
**Officer comment:** This is noted and expanded upon further in the character and appearance section of the report.
- The proposed design would result in the windows of the additional fourth storey (the proposed “third floor” of flats) of the building being larger in size than those on the lower storeys, which would not be typical of the “Georgian” style of design of the existing frontage, which would require the top floor windows to be smaller than those of the lower floors.  
**Officer comment:** The windows proposed on the third floor have been reduced in size following these comments and are now smaller than those on the lower floors.
- The positioning of the proposed new windows in the fourth storey would not be in line with the fenestration of the existing first and second storey of the Bridge Street frontage. This would give an appearance of a haphazard design to the additional storey and would not harmonise with nor enhance the existing building and would be a detriment to the street scene.  
**Officer comment:** The revised drawings ensure that the windows align with the existing windows below.
- It is impossible from the inadequate drawings provided to determine the angle of the proposed slope back from the street frontage elevation that would contain the new windows on the additional top storey, which we can only assume will be some form of dormers.

**Officer comments:** A section (AA) from the South East has been provided for clarification showing the angle of the mansard and small dormer windows.

- The guidance in The Draft Small Sites Design Code states that set-backs should be used to minimise the effect of the top storey of a multistorey building and should be at 1.5 metres in depth. These principles should be applied to this application to add an additional storey to an existing building of a distinctive and pleasing traditional style design in a prominent part of the principal shopping area of Pinner. The additional storey should be subservient to the existing street frontage elevation and be as unobtrusive as possible.

**Officer comment:** The Draft Small Sites Design Code is in draft form and only recently undergone initial rounds of public consultation. The LPA are therefore unable to evaluate the application against this guidance and as such this is not a material consideration in this assessment. However, the revised drawings are considered to be modest in scale and designed as an unobtrusive addition which would appear subservient and congruous with the existing property.

- The current street frontage of the building has regularly spaced fenestration and no “false windows”. The proposed alterations would seek to remove the function of two of the existing second floor windows and brick these in as false windows. This would detract from the original design of the building and would have an adverse effect of the appearance of the building in the street scene.

**Officer comment:** Although the drawings are labelled as ‘faux windows’ these are in fact considered to be ‘blind windows.’ The Council’s Urban Design officer explains that the blind windows are needed because they relate to the internal layout (e.g. they’re concealing internal walls which are located behind this). The only alternative would be to have actual false windows placed on the façade, but these would be visible as not ‘real’ and would be a less architectural solution than the current option. On this basis, the LPA consider what has been proposed the most appropriate solution.

#### 4.5 Statutory and Non-Statutory Consultees

**Planning Policy:**

No Comments Made

**Conservation Officer:**

The building is not identified on the map on page 36 of the CAAMS for ‘Buildings of Townscape Merit Outside of the Conservation Area’.

The existing building is an attractive traditional, neo-Georgian terrace row of shops with flats above.

My view is that the additional height and alterations would be acceptable in principle and preserve the setting of the heritage assets as long as:

- the third floor windows are amended to be in line with the windows below in terms of alignment and their size needs to be reduced so they are shorter than the windows on the levels below in line with the traditional hierarchy of such buildings.

- all details and materials were detailed to match with samples and details to be provided under relevant conditions.

Subject to these amendments, the proposal would be acceptable in my view and preserve the setting of the conservation area.

### **Urban Design**

I think there have been some real improvements over some of the previous street elevation design iterations – but I have some concerns which I think are relatively simple to address.

### **Massing, Scale & Built Form**

1. There is some concern regarding the setback distance of the mansard storey and associated dormer window from the principal street elevation line. Presently there is concern that the setback distance is not of sufficient depth and as such creates an imposing fourth storey massing to the detriment of the existing building frontage character and the character of the wider street scene. Despite the raked mansard roof partially mitigating perceived massing, there is scope to marginally increase this setback distance to further reduce the perception and impact of this additional storey.

### **External Appearance, Composition & Materiality**

2. There have been significant improvements in the proposed street elevation over previous design iterations which is welcome. Specific improvements and ongoing concerns are detailed below.
3. Support for the second floor inset bay elevation amendments including replicating the alignment of sash windows at first floor level and the addition of a blank window centrally to maintain the rhythm of the first floor fenestration at second floor level.
4. Support for protruding brick panels at second floor level, which align with the below fenestration and replicate the existing brick panels which occur on the existing elevation at first floor level below the parapet.
5. Approval of the use of lead cladding to dormer window cheeks at third floor level.
6. The specification of brickwork and clay tiles to match existing use on the street elevation and mansard roof respectively are supported.
7. Confirmation is required as to whether the parapet coping stone specification is to match existing.
8. It is noted and of concern that the vertical distance from second floor windows to parapet coping differs between the southern portion and 33-35 Bridge Street portion of the street elevation. These should be consistent to create a relatively uniform appearance despite the inherent level change as the street elevation progresses northward up Bridge Street.
9. Equally, the vertical distance between the cill of dormer windows and the parapet below, as well as the distance from dormer header to ridge line, should match across the elevation. Currently there are inconsistencies in these setting out distances. It is preferable for these dormer window cills to align with the parapet below, as opposed to being spaced equally within the mansard roof.
10. The three projecting frontages with pediments should continue to slightly project from the two interjacent bays and should not form a continuous elevation as has been shown in drawing 14883-142.

*Further comments made to address above comments:*

Front elevation

1. Positive changes have been made and there is general support for the street elevation. There is alignment between new and existing window openings.
2. One amendment to be made is that of the vertical step of the front elevation (the point where roof, floor and window level rises to accommodate the gradient of Bridge street.) This step should vertically align with the quoins of the pediment-topped bay.

Rear elevation

1. Balconies must feature a full height privacy panel to the northern side of each to prevent overlooking. This should be visible on drawings prior to submission.
2. Welcome to see that the uppermost flight of stair to the roof has been removed. This reduces overall bulk and visibility of the staircase. Any maintenance access could be achieved via a cat ladder.

Amenity

1. Terrace at roof level removed.
2. Amenity space at first floor level (flat roof) has little detail. While a shared amenity space for first and second floor flats would be welcomed in lieu of private amenity to these units, detail on planting and surfacing should be provided as should adequate defensible space adjacent to bedroom windows.

**Historic England**

On the basis of the information provided, we do not consider that it is necessary for this application to be notified to Historic England under the relevant statutory provisions.

**Transport for London (TfL)**

Though we have no objection in principle to the above planning application there are a number of potential constraints on the redevelopment of a site situated close to railway infrastructure. Therefore, we request that the grant of planning permission be subject to conditions to secure a detailed design and method statement to provide details on the use of tall plant, scaffolding and any other lifting equipment to the rear of the property.

**Harrow Conservation Areas Advisory Committee**

The application describes the existing building as being of “low to medium significance” but this is undervaluing it as a representative of the “inter-war parades”, built in the “heyday of the shopping parade”. “Shopping parades are specifically understood...to be planned developments incorporating rows of shops (facing onto an outdoor space), with a strong degree of architectural uniformity.” Historic England, Shopping Parades, Introductions to Heritage Assets.

The proposed design would result in the windows of the fourth storey (the proposed “third floor” of flats) of the building being larger in size than those on the lower storeys, which would not be typical of the “Neo-Georgian” style of design of the existing frontage, which would require the top floor windows to be smaller than those of the lower floors.

The positioning of the proposed new windows in the fourth storey would not be in line with the fenestration of the existing second and third storeys of the Bridge Street frontage. This would give an appearance of a haphazard design to the additional storey and would not harmonise with nor enhance the existing building and would be a detriment to the street scene.

Incorrect plans:

There is no section of the roof to show how it relates to the existing facade, how far it will be set back from the parapet. The effect would be important to any street scene, though we do not think it will affect the conservation area.

The drawing of the proposed Bridge Street elevation is incorrect in several respects, compared with the photograph in the Heritage Statement and should surely be resubmitted, viz: the right-hand bay - the pilasters continue above the eaves, but are not shown so; recessed bay to left of right-hand bay - upper windows are incorrectly aligned; there is no blind window between them; they are not shown as slightly set back from the floor below with their lower panes partly hidden; their sashes are incorrectly shown. Second recessed bay - as for first recessed bay except that sashes appear to be correctly shown.

Comment:

The proposed windows are presumably dormers - there is no cross section or elevation front to back; flat depiction like this does not show how they would look when built; each whole window should be smaller; and a balancing window, real or fake, is needed at the left end of the proposed storey. A corrected elevation is needed.

**Highways Comments:**

The updated Transport Report seems to be acceptable.

**Biodiversity Comments:**

This scheme also can't be supported without revision.

The present application relies on a Preliminary Roost Assessment from April 2021 as before. This was considered acceptable with regard to a previous application for the same site, with only minor caveats, and is still within date.

The current proposals are unlikely to result in any direct impact on features or species of biodiversity importance given the PRA findings and the distance of the site to nearby SINC-designated Pinner Memorial Gardens parkland.

However, the points of concern raised previously appear not to have been addressed and this application falls down on green infrastructure issues.

Although the Planning Statement refers to London Plan Policy G1, the proposals appear not to have addressed this nor the Mayor's Urban Greening Factor requirements. This will require remedy (as was the case with P/2887/21).

If you would otherwise be minded to recommend approval, one option would be to provide additional supporting information prior to determination.

It is not clear from the proposed roof plan whether the roof over the new storey would be flat or pitched. A biodiverse living roof finish to a flat or shallow pitch roof would provide a way to address UGF requirements and provide other benefits. We have no information to be able to determine whether this would be feasible. Should there be no practicable opportunities for providing the necessary greening and biodiversity enhancements within the red line then some degree of offsetting would be mandated.

As it stands, the submitted scheme neither meets UGF requirements nor provides measurable gain for biodiversity within the site boundary. Nor is any acceptable alternative proposed. The proposals would not represent sustainable development on biodiversity grounds - nor with regard to climate change mitigation - and should not be supported without modification. Subject to satisfying the points above, and there being no overriding objections to the development being allowed, any permission that might be granted should be subject to pre-commencement conditions relating to the submission of a Construction Environment Management Plan and Ecological Mitigation and Enhancement Plan.

**Drainage**

Updated details acceptable no further comment.

**Secure by design**

They have now shown control of the fire escapes. If they developer follows all the current SBD guidance I will give them once planning permission is achieved, then it will be fine. Only concern is there is no room for value engineering with this plan.

**5. POLICIES**

- 5.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that:
- 5.2 If regard is to be had to the Development Plan for the purpose of any determination to be made under the Planning Acts, the determination must be made in accordance with the Plan unless material considerations indicate otherwise.
- 5.3 'The Government has issued the National Planning Policy Framework [NPPF 2021] which sets out the Government's planning policies for England and how these should be applied and is a material consideration in the determination of this application.
- 5.4 In this instance, the Development Plan comprises The London Plan 2021 [LP] and the Local Development Framework [LDF]. The LDF comprises The Harrow Core Strategy 2012 [CS], Harrow and Wealdstone Area Action Plan 2013 [AAP], the Development Management Policies Local Plan 2013 [DMP], the Site Allocations Local Plan [SALP] 2013 and Harrow Local Area Map 2013 [LAP]. The relevant policies are referenced within the report below and a summary within Informative 1.

**6. ASSESSMENT**

- 6.1 The main issues are;
  - Principle of the Development
  - Character

- Residential Amenity and Accessibility
- Traffic and Parking
- Flood Risk

## **6.2 Principle of Development**

6.2.1 The relevant policies and guidance are:

- The National Planning Policy Framework (2021)
- The London Plan (2021): H1, H2, H9; H10; SD6; SD7
- Harrow Core Strategy (2012): CS1
- Development Management Policies (2013): DM24, DM35, DM36

6.2.2 The application site is situated within the Pinner Metropolitan Town Centre as outlined in the Harrow Local Plan. Whilst the site is not an allocated development site as defined within the adopted Site Allocations Local Plan (2013), the site is regarded as previously developed land for the purposes of the policies contained within the National Planning Policy Framework and the Harrow Core Strategy which seeks to redirect all new development to the Harrow and Wealdstone Development Opportunity Area, to town centres and to previously developed land in suburban areas. On this basis, the proposal to develop this site for residential purposes is considered to be acceptable in principle.

6.2.3 The London Plan (2021) policy H10 encourages new development to offer a range of housing choices in terms of mix and housing sizes and types. This is reinforced by Core Strategy Policy CS1 which requires new residential development to promote housing choice to meet local needs and to maintain mixed and sustainable communities. Development Management Policy DM24 states that “the appropriate mix of housing will be determined having regard to the location of the site, the character of its surroundings and the need to optimise housing output on previously developed land.”

6.2.4 It is noted that the proposed development seeks to deliver 1 x 2 bed, 3 x 1 bed units. Whilst the Local Planning Authority is unable to prescribe a housing mix for market sale dwellings, it does seek to ensure there is an appropriate housing choice delivered within a scheme, which is in part informed by local evidence base. Although the unit mix is weighted towards lower occupancy, given the town centre location and urban quality and constraints above commercial units it is considered that this site is appropriate for these types of units over larger, family units typically suited to more suburban locations. On this basis, it is considered that the housing mix would be acceptable.

6.2.5 Both retail units and their respective shopfronts would be retained as existing, although internal refurbishment works are proposed for retail unit A (29-31 Bridge Street). It is noted that as part of the works there would be a reduction in retail floorspace by 13sqm to enable the existing external staircase to be rationalised and to provide an area to house the gas and electrics. It is considered that the loss of retail floorspace to facilitate the access to ancillary facilities associated with the new units (which in itself provides a planning benefit to the Borough’s housing stock) would be accepted given that the proposal would not adversely affect the

usability or function of space and would maintain an active frontage ensuring the vitality and viability of the shopping frontage. As such, planning policy have raised no objection to this aspect of the development.

- 6.2.6 Having regard to The London Plan and the Council's policies and guidelines, it is considered that the proposal would provide an increase in smaller housing stock within the Borough as well as maintain the vitality and viability of the Pinner Town Centre, thereby complying with the housing growth objectives and policies of the NPPF, The London Plan, the Harrow Core Strategy, and relevant policies of the 2013 Development Management Policies Local Plan (DMP).

### **6.3 Character of the Area & Impact to the setting of the Conservation Area**

6.3.1 The relevant policies and guidance are:

- The National Planning Policy Framework (2021)
- The London Plan (2021): D3, D4, HC1
- Harrow Core Strategy (2012): CS1.B
- Harrow Development Management Policies (2013): DM1, DM7
- Residential Design Guide SPD (2010)
- The London Plan Housing Supplementary Planning Guidance (2016)
- Code of Practice for Storage and Collection of Refuse and Materials for Recycling in Domestic Properties (2016)
- Pinner High Street Conservation Area Appraisal and Management Strategy
- Historic England Good Practice Advice in Planning Note 3: The setting of heritage assets
- Locally Listed Buildings SPD

6.3.2 Policy DM1 of the DMP requires all new development to provide a high standard of design and layout, respecting the context, siting and scale of the surrounding environment. This policy broadly reflect policy D3 of The London Plan (2021) and gives effect to policy CS1.B of the Harrow Core Strategy 2012, policies which seek to ensure that development respects local character and provide architecture of proportion, composition and scale that enhances the public realm.

6.3.3 The character of the surrounding area, due to the land use, is made up of typical commercial units of various sizes and styles with varied roof features comprising a mix of hipped roofs with front dormer windows, mansard roofs and front gable features. The application property is a three storey building comprising ground floor retail frontage with two storeys of office and residential units. Architecturally it features three front gables with parapet walls behind. Below these are three windows on first and second floor levels respectively. In between the gables at second floor level, there are mansard roofs with two dormer windows.

6.3.4 The proposed development would result in the existing inset walls at second floor level being built up in matching brickwork to appear flush with the existing front wall and installation of timber sash windows across to replicate the existing. The addition of a central 'blind window' to maintain the rhythm of the first floor fenestration at second floor level would be acceptable. It is noted that concerns have been raised about this appearing as a 'faux window' however the urban



design officer states that this is the most appropriate architectural solution to hide the internal wall behind it.

- 6.3.5 The additional storey of accommodation would comprise a mansard roof consisting of clay roof tiles to match the existing and lead clad dormer window cheeks. The entire third floor level would be set back behind a small parapet wall to reduce its visual impact. The dormer windows proposed have been reduced in size following public consultation and been aligned with the windows below. The roofline steps upwards to accommodate the natural ascension in ground level from south to north.

*Impact on setting of the Pinner High Street Conservation Area*

- 6.3.6 This proposal is in the setting of the Pinner High Street Conservation Area and opposite the locally listed 36 High Street. The local list description of 13th March: *'A good example of an inter-war bank which were usually built in a Classical to Neo-Georgian manner to create a prominent landmark on high streets, embodying solidity and respectability. Attractive, red-brick three storey with almost symmetrical, classical to neo-Georgian style frontage. Good original timber casement windows at ground floor level with leaded lights and stone surrounds and key stones, and timber sash windows at first and second floor level. First floor windows have original gauged brick arches above them. The roof is steeply sloped framed by short chimney stacks either side. A key feature is the untouched stone canopy porch above the entrance door with decorative, classical supporting brackets.'*
- 6.3.7 The building is not identified on the map on page 36 of the CAAMS for 'Buildings of Townscape Merit Outside of the Conservation Area'. The existing building is an attractive traditional, neo-Georgian terrace row of shops with flats above.
- 6.3.8 The Conservation Officer has been notified of the application and confirms that the additional height and alterations would be acceptable in principle and would preserve the setting of the heritage assets so long as there is a reduction in the size of the windows and alignment of windows below and details would match with samples to be provided under relevant conditions. Similar comments were made by the Conservation Area Advisory Committee (CAACs). As noted above, the necessary amendments were made to overcome heritage concerns raised and the recommended conditions have been attached.
- 6.3.9 The proposed extensions would reflect the style of the building and as such would provide a harmonious extension. A condition is attached to the permission requiring the external materials to match the existing finishes of the building.

*External changes to the rear*

- 6.3.10 An external staircase is proposed to the rear of the building behind the existing offices at first and second floor level. Fenestration changes to facilitate would involve the blocking up of two office windows currently serving non-habitable rooms which would be acceptable on balance. Use of the existing access to enter the proposed units in Block A would also be acceptable.

### *Refuse Storage*

- 6.3.11 The existing arrangements for refuse for the retail units would be maintained. However, a refuse store for additional two commercial waste bins would be provided at ground floor level for the additional units.

### *Landscaping and Biodiversity*

- 6.3.12 It is noted that there is limited opportunity for soft landscaping due to the site constraints and circumstances. The Council's Biodiversity Officer acknowledges that the current proposals are unlikely to result in any direct impact on features or species of biodiversity importance given the PRA findings and the distance of the site to nearby SINC-designated Pinner Memorial Gardens parkland and acknowledges the acceptability of the Preliminary Roost Assessment provided in support of the application. As this is a minor application it would not trigger the need to comply with Urban Greening Factor requirements. Notwithstanding this, Policy G1 seeks developments to incorporate elements of green infrastructure which are integrated into London's wider green. On this basis the applicants have accepted a pre-commencement condition securing proportionate biodiversity gain.

### *Fire Safety*

- 6.3.13 Part A of Policy D12 of the London Plan (2021), requires the demonstration of suitably positioned and unobstructed space for fire appliances and evacuation assembly points, and that developments ensure robust strategies for evacuation are in place as well as confirmation of the fire-fighting water supply. A Fire Safety Statement has not been submitted with this application. As such, a pre commencement condition is attached to this permission which would require the submission of this document in a further application before commencement.
- 6.3.14 Subject to the conditions mentioned above, it is considered that the resultant development would be appropriate in its context and would comply with policy D3 and HC1 of The London Plan (2021), Core Policy CS1(B) of the Harrow Core Strategy and policy DM1 of the Council's Development Management Policies Local Plan.

## **6.4 Residential Amenity**

- 6.4.1 The relevant policies and guidance are:
- The London Plan (2021): D3, D6
  - Harrow Core Strategy (2012): CS1
  - Harrow Development Management Policies (2013): DM1, DM2, DM27
  - Residential Design Guide (2010)
  - The London Plan Housing Supplementary Planning Guidance (2016)

### *Impact on neighbouring properties*

- 6.4.2 Policy DM1 of the DMP seeks to ensure that "proposals that would be detrimental to the privacy and amenity of neighbouring occupiers, or that would fail to achieve satisfactory privacy and amenity for future occupiers of the development, will be

resisted”.

- 6.4.3 Given that the changes proposed would relate to internal reconfigurations and an additional storey confined to the existing roof it is not considered that the proposed development would give rise to any undue impact in terms of loss of daylight, sunlight or overshadowing to neighbouring properties.
- 6.4.4 The front facing windows and balconies would face onto the main road on Bridge Street and the rear facing windows would face onto the service yard of the retail units fronting Bridge Street. As such, there would not undue impact with regards to overlooking and loss of privacy to neighbouring properties.
- 6.4.5 Accordingly, it is considered that the proposed development would not unacceptably harm the amenities of surrounding occupiers through a loss of light, privacy, overlooking or perception of overlooking and would therefore would accord with the aims and objectives of policy D3 of The London Plan (2021), Core Policy CS1B of the Harrow Core Strategy (2012) and policy DM1 of the Harrow Development Management Policies Plan (2013).

*Proposed Quality of accommodation*

- 6.4.6 Policy D6 of the London Plan specifies that boroughs should ensure that, amongst other things, ‘housing development should be of high quality design and provide adequately-sized rooms with comfortable and functional layouts’. It also sets out the minimum internal space standards for new dwellings. The use of these residential unit GIA’s as minima is also reiterated in Appendix 1 of the Residential Design Guide SPD.
- 6.4.7 The proposed development includes the following:

<b>Apartment</b>	<b>Floorspace (GIA)</b>	<b>Beds</b>	<b>Persons</b>
<b>Existing</b>			
A1	35.5 sqm	1	1
A2	56.7 sqm	1	2
B2	42.9 sqm	1	1
A3	45.4 sqm	1	1
A4	51.3 sqm	1	2
B4	60.7 sqm	1	1
<b>Proposed</b>			
A5	37.8 sqm	1	1
A6	45.4 sqm	1	1
B5	62.4 sqm	2	3
B6	51.1 sqm	1	1

- 6.4.8 The proposed dwellings would all meet or exceed the minimum floor space requirements in terms of gross internal floor area, in terms of storage and in terms of minimum bedroom sizes. Three out of the four flats would be dual aspect and all habitable rooms would experience high levels of outlook and daylight. in accordance with policy DM1 and the adopted Residential Design Guide SPD. It is

noted that the balconies are close to one another and therefore it is considered necessary to include a condition requesting privacy screens to protect the residential amenity of future occupiers.

#### *Amenity Space*

- 6.4.9 The applicant does propose to provide balconies for all 1 bed units which meet the minimum 5 sqm requirement of the London Plan. It is not that the 2 bedroom unit would include the provision of private amenity space due to the introduction of the external stairwell. Notwithstanding this, having regard to the town centre location and the presence of the existing flats which do not have access to any amenity space, this arrangement is considered to be acceptable in this instance.
- 6.4.10 The stacking arrangements are such that there is some overlapping of kitchen and living spaces over bedrooms. The applicant has been made aware of this and that additional noise transference measures would be required for submission to building control. Building regulations would cover any works to ensure noise transference is kept to a minimum.

#### *Secure by design*

- 6.4.11 Proposed Units A5 and A6 would be accessed via the rationalised stairwell at ground floor and through the existing entrance to Block A at first floor level to the rear and via the existing internal staircase, which is compatible with the existing arrangement. The new units within Block B would also be accessed via the common stairway accessed off Bridge Street. Although the commercial units use the staircase to access the offices there will be gated access to the third floor level to the residential floors.
- 6.4.12 The Designing Out Crime Officer has been consulted on the proposals, and following these discussions the plans were amended to ensure SBD fences and gates to restrict access to the first floor at ground and first floor levels; all lockers in the communal area as well as fenced bin storage in the communal area. The cycle hoops have been moved to the rear. A secure by design condition prior to occupation will confirm that all the measures outlined have been built out as per the approved plans.

#### *Accessible Homes*

- 6.4.13 Policy DM2 of the DMP seeks to ensure that all new housing is built to 'Lifetime Homes' standards. Policy CS1.K of the Harrow Core Strategy requires all new dwellings to comply with the requirements of Lifetime Homes.
- 6.4.14 The Technical Housing Standards (March 2015) altered the interpretation of the relevant London Plan (2021) policies and associated Housing SPG standards, placing the previous Lifetime Homes Standards under the control of Building Control regime (specifically Part M of the Building Regulations).
- 6.4.15 It is acknowledged that the flats are at upper floor levels and so would not be suitable for wheelchair users. A condition to ensure that the proposed dwellings will meet regulation M4 (1) as far as possible is included within this permission

## 6.5 Traffic and Parking

6.5.1 The relevant policies are:

- The London Plan (2021): T4, T5, T6
- Harrow Core Strategy: CS1
- Harrow Development Management Policies Local Plan (2013): DM1, DM42

6.5.2 A transport statement has been provided by Paul Mew Associates Limited on behalf of the applicant. The site is located in Pinner, to the north-west of the borough, in an area with a base Public Transport Accessibility Level (PTAL) of 3 (moderate) which is predicted to rise to 4 (Good). Bridge Street forms the main shopping area of Pinner town centre. The Metropolitan Line runs immediately to the south of the site. In the immediate vicinity are various shops, supermarkets, gym, restaurants and access to four regular bus services and Pinner Underground station (Metropolitan). The site currently consists of ground floor shops, and eight flats across the first and second floors. No dedicated parking is provided.

### *Access and Parking Provision*

6.5.3 The existing service yard is proposed to be used for refuse collections as is the current arrangement. No car parking is proposed and the Transport Statement states that the application will agree to a legal agreement (s106) restricting future residents from obtaining permits for the surrounding Controlled Parking Zone (CPZ). This would also require a contribution of £1500 to amend the relevant Traffic Management Order (TMO). As the proposal site has good access to public transport and amenities, a car-free development is considered to be acceptable. A disabled bay is located to the rear of the site to cater for ambulant disabled visitors to the site.

### *Cycle Storage*

6.5.4 The London Plan 2021 requires a minimum of 7 long stay and 2 short stay spaces for this proposal. It is intended to provide four lockers for residential use and 2 for visitor use behind the stairwell at ground floor level and a further five at the rear. All cycle parking is located at ground floor level for accessibility purposes which would support the car free proposals.

### *Delivery and Servicing*

6.5.5 The proposals for refuse collections do not present any highways concern, however, given that the development would use the existing arrangements in place it is considered to be acceptable in this regard. The Highways Authority notes that the frequency of deliveries that may be generated by a development of this size is not expected to be high therefore, would not have a significant impact on the operations of the highway network. Notwithstanding this, these could be carried out at the rear of the property or alternatively, there is a loading bay on Bridge Street, a short distance from the site.

### *Trip Generation*

6.5.6 The Transport Statement includes a TRICS analysis of potential trip generation of the development. The provided information is limited however, indicates a total of 27 two-way person trips per day would be generated by the site. It is assumed that these trips are mainly made up of walking, cycling and public transport modes as there is no on-site parking and limited options for on-street parking. The Highways Authority did not raise concerns that this would result in any highway impact or safety concerns.

#### *Summary*

6.5.7 An additional four flats are unlikely to result in a noticeable impact in terms of highways. Should future residents choose to drive, they would have to find parking away from the site and would be dispersed throughout the local area. This proposal is not considered to bring about any highway safety concerns. Cycle parking arrangements however would need to be revised to ensure these are appropriate and should include short stay provision within the site if possible. Subject to pre-commencement conditions relating to cycle storage facilities, submission of a construction logistics plan and a legal agreement and contribution will be required in relation to the parking permit restriction the application is considered to be acceptable.

## **6.6 Flood Risk and Drainage**

6.6.1 The relevant policies and guidance are:

- The National Planning Policy Framework (2021)
- The London Plan (2021): SI 13
- Harrow Core Strategy (2012): CS1
- Harrow Development Management Policies (2013): DM9, DM10

6.6.2 The proposed development is located within Flood Zone 2 according to the Environment Agency Flood Map. A Flood Risk Assessment was provided and the Drainage Engineer at the Council raised no objections. However, the applicant was requested to consider installation of green/blue roof to reduce surface water run-off from the site. Suitable biodiversity measures which may be appropriate to improved drainage will be considered as part of the ecological enhancements subject of pre-commencement conditions. A drainage strategy was also requested and included within an updated Flood Risk Assessment. Accordingly, the proposal would be acceptable and would not give rise to Flood Risk issues.

## **7. CONCLUSION AND REASONS FOR APPROVAL**

7.1 The proposal would contribute to the variety of housing stock in the borough. The proposed development would appropriately relate to the site, local context, massing and architectural appearance and would bring forward housing provision of a satisfactory layout and design to ensure that the future occupiers would benefit from an acceptable standard of living accommodation.

7.2 Given the location of the application site, Officers are satisfied that the proposal would maintain an appropriate quality of residential amenity for the adjoining

occupiers. Furthermore, the transport aspects of this proposal are considered to be in accordance with strategic and local transport policies.

- 7.3 For all the reasons considered above, and weighing up the development plan policies and proposals, and other material considerations including comments received in response to notification and consultation as set out above, this application is recommended for grant.

## **APPENDIX 1: Conditions and Informatives**

### **Conditions**

#### **1      Timing**

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

REASON: To comply with the provisions of Section 91 of the Town and Country Planning Act 1990.

#### **2      Approved Drawing and Documents**

The development hereby permitted shall be carried out in accordance with the following approved plans and documents: 14883-101C Existing Block Plan 14883-102C Existing Site Plan; 14883-103A Existing Floor Plans; 14883-110E Location Plan; 14883-112A Existing Elevations; 14883-131H Proposed Block Plan; 14883-132H Proposed Site Plan; 14883-141U Proposed Floor Plans; 14883-142K Proposed Elevations; 14883-143J Proposed Elevations; 14883-144H Proposed Roof Plan; 14883-145C Proposed Circulation Area; P2004 Bridge Street Transport Statement v8 210222; Bridge Street Preliminary Roost Assessment 08 04 21; 91041 Harris Bridge St V1.0 – 160921 FRA; 20210627 Air Quality Report Bridge Street Pinner\_V1Rev B; Bridge Street 29-35 Pinner-Heritage Statement 10.06.20212022.06.30 Planning Statement (Rev A); 14883-DAPA01 - Pinner December 2021\_Rev G

REASON: For the avoidance of doubt and in the interests of proper planning.

#### **3      Construction Method Statement**

No development shall take place, including any works of demolition, until a Detailed Construction Logistics Plan has been submitted to, and approved in writing by, the local planning authority in accordance with the format and guidance provided by Transport for London – [www.tfl.gov.uk](http://www.tfl.gov.uk). The Detailed Construction Logistics Plan shall provide for:

- a) Parking of vehicles of site operatives/visitors;
- b) HGV access to site – loading and unloading of plant and materials;
- c) Number of HGV's anticipated;
- d) Storage of plant and materials used in constructing the development;
- e) Programme of work and phasing;
- f) Site layout plan;
- g) Highway condition (before, during, after);
- h) Measures to control dust and dirt during construction;
- i) A scheme for recycling/disposing of waste resulting from demolition and construction works; and
- j) details showing the frontage/ the boundary of the site enclosed by site hording to a minimum height of 2 metres.



The development shall be carried out in accordance with the approved Detailed Construction Logistics Plan, or any amendment or variation to it as may be agreed in writing by the local planning authority.

REASON: To minimise the impacts of construction upon the amenities of neighbouring occupiers and to ensure that development does not adversely affect safety on the transport network and to ensure that the transport network impact of demolition and construction work associated with the development is managed, this condition is a PRE-COMMENCEMENT condition.

#### 4 Cycle Details

Prior to commencement of development, revised details of shelters, racks, dimensions and location of a minimum of seven long stay and two short stay cycle parking spaces shall be submitted to and approved in writing by the local planning authority. The cycle storage shall be made available prior to occupation and shall be retained thereafter.

REASON: To ensure the satisfactory provision of safe cycle storage facilities, to provide facilities for all the users of the site and in the interests of highway safety and sustainable transport.

#### 5 Construction Environment Management Plan

Prior to commencement, the applicant will provide to the Council for approval in writing

1. A proportionate Construction Environment Management Plan that, as well as covering other relevant environmental considerations, will set out how contractors will be made aware and have regard to protected species and the practical and legal situation with regard to development. The CEMP should at least ensure that the procedures set out in the approved Preliminary Roost Assessment report (PEAR) will be implemented in full and give appropriate consideration to breeding birds.
2. A lighting plan detailing measures to be taken to minimise potential impacts on bats and other nocturnal species, with lighting provision to be installed, operated and managed in accordance with current Bat Conservation Trust guidance.
3. An Ecological Mitigation and Enhancement Plan (EMEP) to provide full details, plans and specifications for the onsite and/or offsite measures to be implemented to provide appropriate biodiversity gain and green infrastructure enhancement to meet relevant national, London and local planning policy obligations, with specific regard to achieving London urban greening standards. This should include details of the wildlife shelter provision proposed in the PEAR and/or Planning Statement as well as incorporating.

The plans should provide clear details of what is proposed and their implementation.

REASON: To provide enhancement for biodiversity in accordance with the requirements of the National Planning Policy Framework, the London Plan and local plan policies DM20 and DM21.

6 Fire Safety

Prior to the commencement of the development hereby approved, a Fire Safety Statement shall be submitted to and approved in writing by the Local Planning Authority, this statement shall include details of how the development will function in terms of the following:

- 1) identify suitably positioned unobstructed outside space: a) for fire appliances to be positioned on b) appropriate for use as an evacuation assembly point
- 2) is designed to incorporate appropriate features which reduce the risk to life and the risk of serious injury in the event of a fire; including appropriate fire alarm systems and passive and active fire safety measures
- 3) is constructed in an appropriate way to minimise the risk of fire spread
- 4) provide suitable and convenient means of escape, and associated evacuation strategy for all building users
- 5) develop a robust management strategy for evacuation which is to be periodically updated and published (details of how often this management strategy is to be reviewed and published to be included), and which all building users can have confidence in
- 6) provide suitable access and equipment for firefighting which is appropriate for the size and use of the development.

The development shall be operated in accordance with the approved details in perpetuity.

REASON: To ensure that the fire safety of the proposed building is managed in a satisfactory manner and that the development contributes to fire safety in line with Policy D12A of the London Plan (2021). To ensure appropriate fire safety measures are approved before development commences on site, this condition is a PRE-COMMENCEMENT condition.

7 Materials

Notwithstanding the details shown on the approved drawings, prior to any construction at third floor level samples of the materials (or appropriate specification) to be used in the construction of the external surfaces noted below shall be submitted to, and agreed in writing by, the local planning authority:

- a) facing materials for the buildings;
- b) windows/ doors/glazing; and
- c) balconies (including privacy screens).

The development shall be carried out in accordance with the details so agreed and shall be retained as such thereafter.

REASON: To ensure that the development is carried out to the highest standards of architecture and materials.

8 Refuse Storage

The refuse and waste bins shall be stored at all times, other than on collection days, within the designated refuse storage areas as shown on the approved plans.

REASON: To enhance the appearance of the development and safeguard the character and appearance of the area.

9 Part M Dwellings

The development hereby permitted shall be constructed to the specifications of: "Part M, M4 (1), Category 1: VISIBLE Dwellings" of the Building Regulations 2013 and thereafter retained in that form.

REASON: To ensure that the development is capable of meeting 'Accessible and Adaptable Dwellings' standards.

10 Use of flat roof

The flat roof areas of the building beyond any balconies or terraces, hereby permitted shall not be used as a balcony, roof garden or similar amenity area without the grant of further specific permission from the local planning authority.

REASON: To safeguard the amenity of neighbouring residents.

11 Restrictions to telecommunication Items

Notwithstanding the provisions of the Electronic Communications Code Regulation 5 (2003) in accordance with The Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking or re-enacting that order with or without modification), no development which would otherwise fall within Schedule 2, Part 16, Class A of that order shall be carried out in relation to the development hereby permitted without the prior written permission of the local planning authority.

REASON: In order to prevent the proliferation of individual telecommunication items on the building which would be harmful to the character and appearance of the building and the visual amenity of the area.

## **Informatives**

### **1 Relevant Policies**

The following policies and guidance are relevant to this decision:  
National Planning Policy and Guidance:

#### **National Planning Policy Framework (2021)**

##### **The London Plan (2021):**

GG2 Making the best use of land  
GG4 Delivering the homes Londoners need  
SD6 Town centres and high streets  
HC1 Conservation and heritage  
H1 Increasing housing supply  
H2 Small Sites  
H10 Housing Size mix  
D3 Optimising site capacity through the design-led approach  
D6 Housing quality and standards  
D7 Accessible housing  
E9 Retail, markets and hot food takeaways  
T4 Assessing and Mitigating transport impacts  
T5 Cycling  
T6 Car Parking  
T6.1 Residential Parking  
T7 Deliveries, servicing and construction  
SI 12 Flood Risk Management

Harrow Core Strategy (2012):

CS1

Development Management Policies Local Plan (2013):

DM1, DM2, DM7, DM20, DM21, DM22, DM23, DM24, DM27, DM31, DM38, DM42, DM43, DM44, DM45, DM50

Adopted Supplementary Planning Documents:

London Plan Housing Supplementary Planning Guidance (2016)

Supplementary Planning Document: Residential Design Guide (2010)

Code of Practice for Storage and Collection of Refuse and Materials for Recycling in Domestic Properties (2016)

Pinner High Street Conservation Area Appraisal and Management Strategy

Historic England Good Practice Advice in Planning Note 3: The setting of heritage assets

Locally Listed Buildings SPD

### **2 Pre-application engagement**

Statement under Article 31 (1)(cc) of The Town and Country Planning (Development Management Procedure) (England) Order 2010 (as amended)  
This decision has been taken in accordance with paragraphs 187-189 of The National Planning Policy Framework. Pre-application advice was sought and provided and the submitted application was in accordance with that advice.

3 Compliance with Planning Conditions

Compliance With Planning Conditions Requiring Submission and Approval of Details Before Development Commences

- You will be in breach of planning permission if you start development without complying with a condition requiring you to do something before you start. For example, that a scheme or details of the development must first be approved by the Local Planning Authority.
- Carrying out works in breach of such a condition will not satisfy the requirement to commence the development within the time permitted.
- Beginning development in breach of a planning condition will invalidate your planning permission.
- If you require confirmation as to whether the works you have carried out are acceptable, then you should apply to the Local Planning Authority for a certificate of lawfulness.

4 Considerate Contractor Code Of Practice

The applicant's attention is drawn to the requirements in the attached Considerate Contractor Code of Practice, in the interests of minimising any adverse effects arising from building operations, and in particular the limitations on hours of working.

5 Party Wall Act:

The Party Wall etc. Act 1996 requires a building owner to notify and obtain formal agreement from adjoining owner(s) where the building owner intends to carry out building work which involves:

1. work on an existing wall shared with another property;
  2. building on the boundary with a neighbouring property;
  3. excavating near a neighbouring building,
- and that work falls within the scope of the Act.

procedures under this Act are quite separate from the need for planning permission or building regulations approval.

"The Party Wall etc. Act 1996: Explanatory booklet" is available free of charge from:

Communities and Local Government Publications, PO Box 236, Wetherby, LS23 7NB

Please quote Product code: 02 BR 00862 when ordering

Also available for download from the CLG website:

<http://www.communities.gov.uk/documents/planningandbuilding/pdf/133214.pdf>

Tel: 0870 1226 236 Fax: 0870 1226 237

Textphone: 0870 1207 405

E-mail: [communities@twoten.com](mailto:communities@twoten.com)

6 Sustainable Urban Drainage

The applicant is advised that surface water run-off should be controlled as near to its source as possible through a sustainable drainage approach to surface water management (SUDS). SUDS are an approach to managing surface water

run-off which seeks to mimic natural drainage systems and retain water on or near the site as opposed to traditional drainage approaches which involve piping water off site as quickly as possible.

SUDS involve a range of techniques including soakaways, infiltration trenches, permeable pavements, grassed swales, ponds and wetlands. SUDS offer significant advantages over conventional piped drainage systems in reducing flood risk by attenuating the rate and quantity of surface water run-off from a site, promoting groundwater recharge, and improving water quality and amenity. Where the intention is to use soak ways they should be shown to work through an appropriate assessment carried out under Building Research Establishment (BRE) Digest 365. Support for the SUDS approach to managing surface water run-off is set out in the National Planning Policy Framework (NPPF) and its accompanying technical guidance, as well as the London Plan. Specifically, the NPPF (2012) gives priority to the use of sustainable drainage systems in the management of residual flood risk and the technical guidance confirms that the use of such systems is a policy aim in all flood zones. Policy 5.13 of the London Plan (2012) requires development to utilise sustainable drainage systems unless there are practical reasons for not doing so. Sustainable drainage systems cover the whole range of sustainable approaches to surface drainage management. They are designed to control surface water run-off close to where it falls and mimic natural drainage as closely as possible. Therefore, almost any development should be able to include a sustainable drainage scheme based on these principles. The applicant can contact Harrow Drainage Section for further information.

## 7 Liability For Damage To Highway

The applicant is advised to ensure that the highway is not interfered with or obstructed at any time during the execution of any works on land adjacent to a highway. The applicant is liable for any damage caused to any footway, footpath, grass verge, vehicle crossing, carriageway or highway asset. Please report any damage to [nrswa@harrow.gov.uk](mailto:nrswa@harrow.gov.uk) or telephone 020 8424 1884 where assistance with the repair of the damage is available, at the applicant's expense. Failure to report any damage could result in a charge being levied against the property.

## 8 Street Numbering

Harrow Council is responsible for the naming and numbering of new or existing streets and buildings within the borough boundaries. The council carries out these functions under the London Government Act 1963 and the London Building Acts (Amendment) Act 1939. All new developments, sub division of existing properties or changes to street names or numbers will require an application for official Street Naming and Numbering (SNN). If you do not have your development officially named/numbered, then then it will not be officially registered and new owners etc. will have difficulty registering with utility companies etc. You can apply for SNN by contacting [technicalservices@harrow.gov.uk](mailto:technicalservices@harrow.gov.uk) or on the following link. [http://www.harrow.gov.uk/info/100011/transport\\_and\\_streets/1579/street\\_naming\\_and\\_numbering](http://www.harrow.gov.uk/info/100011/transport_and_streets/1579/street_naming_and_numbering)

9 Mayoral CIL

Please be advised that approval of this application (either by Harrow Council, or subsequently by the Planning Inspectorate if allowed on appeal following a refusal by Harrow Council) will attract a Community Infrastructure Levy (CIL) liability, which is payable upon the commencement of development. This charge is levied under s.206 of the Planning Act 2008 Harrow Council, as CIL collecting authority, has responsibility for the collection of the Mayoral CIL. The Provisional Mayoral CIL liability for the application, based on the Mayoral CIL levy rate for Harrow of £60/sqm is £11,802

You are advised to visit the planning portal website where you can download the appropriate document templates. Please complete and return the Assumption of Liability Form 1 and CIL Additional Information Form 0.

[https://ecab.planningportal.co.uk/uploads/1app/forms/form\\_1\\_assumption\\_of\\_liability.pdf](https://ecab.planningportal.co.uk/uploads/1app/forms/form_1_assumption_of_liability.pdf)

[https://ecab.planningportal.co.uk/uploads/1app/forms/cil\\_questions.pdf](https://ecab.planningportal.co.uk/uploads/1app/forms/cil_questions.pdf)

If you have a Commencement Date please also complete CIL Form 6:

[https://ecab.planningportal.co.uk/uploads/1app/forms/form\\_6\\_commencement\\_notice.pdf](https://ecab.planningportal.co.uk/uploads/1app/forms/form_6_commencement_notice.pdf)

The above forms should be emailed to [HarrowCIL@Harrow.gov.uk](mailto:HarrowCIL@Harrow.gov.uk)

Please note that the above forms must be completed and provided to the Council prior to the commencement of the development; failure to do this may result in surcharges and penalties

10 Harrow CIL

Harrow has a Community Infrastructure Levy which applies Borough wide for certain developments of over 100sqm gross internal floor space.

Harrow's Charges are:

Residential (Use Class C3) - £110 per sqm;

Hotels (Use Class C1), Residential Institutions except Hospitals, (Use Class C2), Student Accommodation, Hostels and HMOs (Sui generis) - £55 per sqm;

Retail (Use Class A1), Financial & Professional Services (Use Class A2), Restaurants and Cafes (Use Class A3) Drinking Establishments (Use Class A4) Hot Food Takeaways (Use Class A5) - £100 per sqm

All other uses - Nil.

The Provisional Harrow CIL liability for the application, based on the Harrow CIL levy rate for Harrow of £110/sqm is £30,291.80

This amount includes indexation which is 326/224. The floorspace subject to CIL may also change as a result of more detailed measuring and taking into account any in-use floor space and relief grants (i.e. for example, social housing).

The CIL Liability is payable upon the commencement of development.

You are advised to visit the planningportal website where you can download the relevant CIL Forms.

Please complete and return the Assumption of Liability Form 1 and CIL Additional Information Form 0.

[https://ecab.planningportal.co.uk/uploads/1app/forms/form\\_1\\_assumption\\_of\\_liability.pdf](https://ecab.planningportal.co.uk/uploads/1app/forms/form_1_assumption_of_liability.pdf)

[https://ecab.planningportal.co.uk/uploads/1app/forms/cil\\_questions.pdf](https://ecab.planningportal.co.uk/uploads/1app/forms/cil_questions.pdf)

If you have a Commencement Date please also complete CIL Form 6:

[https://ecab.planningportal.co.uk/uploads/1app/forms/form\\_6\\_commencement\\_notice.pdf](https://ecab.planningportal.co.uk/uploads/1app/forms/form_6_commencement_notice.pdf)

The above forms should be emailed to [HarrowCIL@Harrow.gov.uk](mailto:HarrowCIL@Harrow.gov.uk)

Please note that the above forms must be completed and provided to the Council prior to the commencement of the development; failure to do this may result in surcharges

- 11      INFORM67 - Fire Safety Statement. The submission/approval of the Fire Safety Statement does not replace the need for building regulation approval in relation to fire safety, nor does it convey or imply any approval under those regulations.

**Checked**

Interim Head of Development Management	DM	06/07/2022
Corporate Director	DP	07/07/2022



**APPENDIX 2: SITE PLAN**



## APPENDIX 3: SITE PHOTOGRAPHS

Front Elevation



Existing Staircase to Upper floor flats and offices





Rear of Property





View from inside property to rear service yard



View of rear service yard looking towards the property



View towards 21B in the rear service yard



Service yard access from Chapel Way





# APPENDIX 4: PLANS AND ELEVATIONS

## Existing Plans



GROUND FLOOR  
scale 1:200



FIRST FLOOR  
scale 1:200



SECOND FLOOR  
scale 1:200



ROOF LAYOUT  
scale 1:200

# Proposed Plans



PROPOSED GROUND FLOOR  
scale 1:200



PROPOSED FIRST FLOOR  
scale 1:200

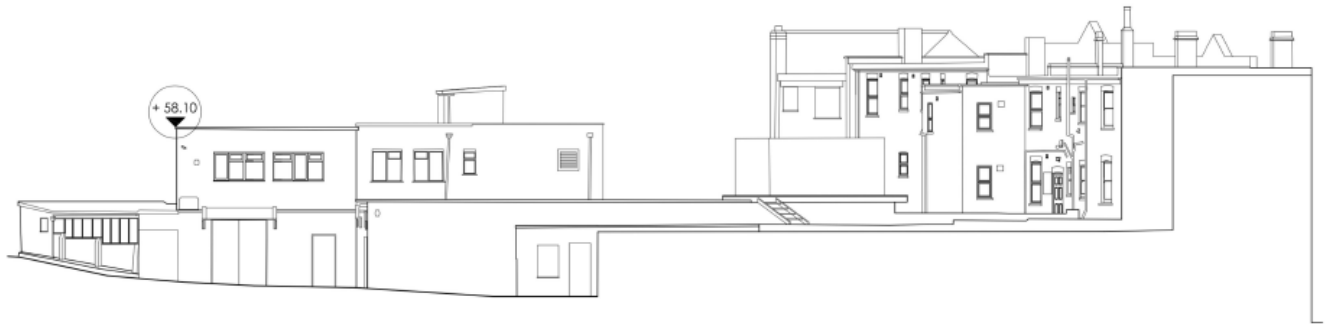


PROPOSED SECOND FLOOR  
scale 1:200



PROPOSED THIRD FLOOR  
scale 1:200

# Existing Elevations



ELEVATION B: SOUTH - EAST ELEVATION  
scale 1:200



A
Rev

## Rear Elevation (South-west)



# Proposed Elevations

## Proposed Front elevation

**KEY**

- 1. BRICKWORK - TO MATCH EXISTING
- 2. DORMERS - LEAD CHIEKS
- 3. ROOF TILES - TO MATCH EXISTING
- 4. WINDOWS - TIMBER TO MATCH EXISTING
- 5. REAR WALL - WHITE RENDER
- 6. STRINGER - PROTRUDING BRICK COURSE
- 7. PANEL - PROTRUDING BRICK
- 8. FAUX WINDOW - INSET BRICK
- 9. GLAZING - OBSCURED

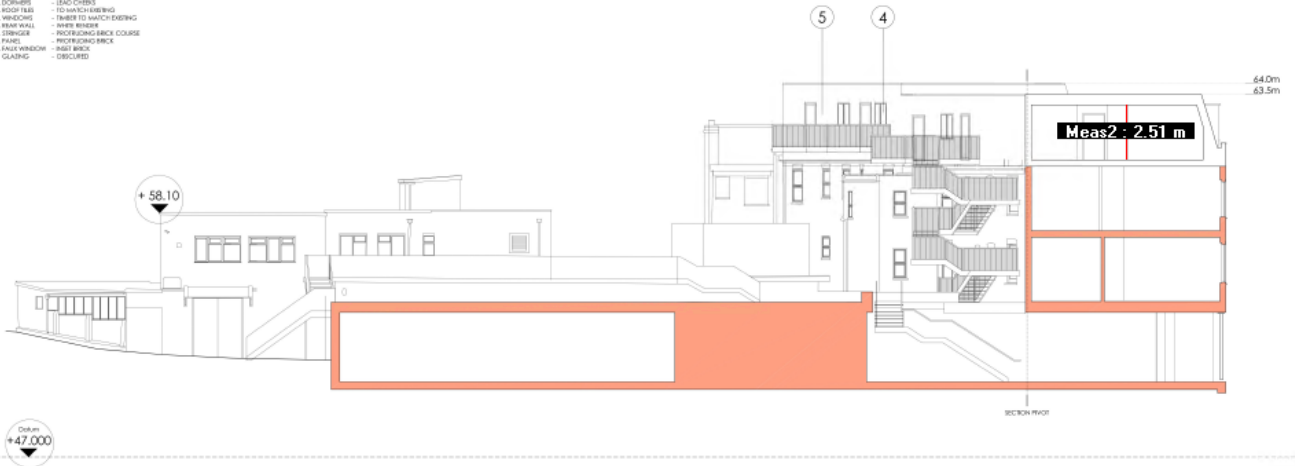


## Proposed Rear Elevation

- 3. ROOF TILES - TO MATCH EXISTING
- 4. WINDOWS - TIMBER TO MATCH EXISTING
- 5. REAR WALL - WHITE RENDER
- 6. STRINGER - PROTRUDING BRICK COURSE
- 7. PANEL - PROTRUDING BRICK
- 8. FAUX WINDOW - INSET BRICK
- 9. GLAZING - OBSCURED



- KEY
- 1. BRICKWORK - TO MATCH EXISTING
- 2. DOORCASE - TO MATCH EXISTING
- 3. ROOF FILL - TO MATCH EXISTING
- 4. WINDING - TO MATCH EXISTING
- 5. STAIR WALL - TO MATCH EXISTING
- 6. STAIRCASE - TO MATCH EXISTING
- 7. FLOOR - TO MATCH EXISTING
- 8. GLAZING - TO MATCH EXISTING
- 9. GLAZING - TO MATCH EXISTING



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